

3.2 0-2L Construction

3.2-1 Intent

0-2L Class is designed as a budget class and competitors are expected to respect the spirit of these regulations. Only the modifications mentioned are permissible. IF IT IS NOT MENTIONED IT IS NOT PERMITTED.

3.2.2 Engine

Engine is to match car type, make and model as per factory specifications.

No forced induction type engines or Rotary's.

No aftermarket computers.

3.2.3 Driveline

Diffs may be locked. No four wheel drives,

3.2.4 Tyres

Maximum tyre size 205 or equivalent road tyre only.

Racing tyres with exception- of rally tyres allowed for non class racing.

(Rally tyres ONLY in sprint series).

3.2.5 Exhaust I Mufflers

Extractors allowed as optional.

The Exhaust pipe must exit behind the driver's seat or further and must be directed away from the fuel tank. To have maximum height to top of pipe outlet to be 300mm above ground level.

All participating cars are to run mufflers, Max 95 db.

3.2.6 Suspension I Brakes

Suspension and brakes may be upgraded but standard type of suspension to remain.

3.2.7 Steering / Interior

All steering locks to be removed

All plastic and interior trim to be removed with the exception of the dashboard and front seats. The dashboard may be replaced and the seats can be replaced by a race type or safer seat.

3.2.8 Kill Switch and Battery Isolation Switch

All classes must be fitted with a kill switch which must stop the engine and turn off all other electrical equipment.

On and Off positions must be clearly marked.

The Alternator lead if live must be protected by a maximum 100amp Fuse, fitted near the Battery end of cable, as close as possible.

The Kill Switch must be fitted within reach of the driver when belted in.

Batteries may be relocated but must be mounted away from the fuel tank.

3.2.9 Windscreen I Glass

All exterior lamps to be removed, removing glass is optional. (Other Clubs running this class, run without glass)

The entire front screen opening to be covered by a steel mesh max opening of 75mm x 75mm with 2.6mm min wire size.

3.2.10 Panels

All panels to remain uncut

After market flares, skirts and spoilers may be fitted.

3.2.11 Fueltank

Standard fuel tanks may be replaced.

3.2.12 Rollcage

Full roll cages are compulsory, minimum specifications for roll cages being:

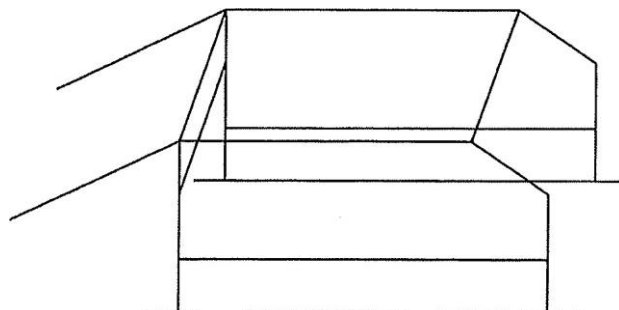
3mm steel wall piping with a minimum of 38mm outside diameter for main hoop and a minimum of 32mm outside diameter, 3mm steel wall piping for lateral supports (galvanised pipe not recommended).

There must be 50mm clearance between top and side of driver's helmet and roll cage or roof.

All fixing plates are to be a minimum of 5mm thick and not less than 100mmx100mm, or to scrutineers discretion. The point at which roll cages are mounted must be of sound condition and to the scrutineers satisfaction. All cages must have support bars across top of front windscreen and also between main hoop of cage 2/3rds from base of seat, and no more than 75mm clearance between rear of seat and bar (in case of seat failure). Other recognised cages accepted at scrutineers discretion. Scrutineers decision is final.

Bends formed in roll cage pipe must be smooth and uniform and to be not less than 0.9 of the original pipe OD size.

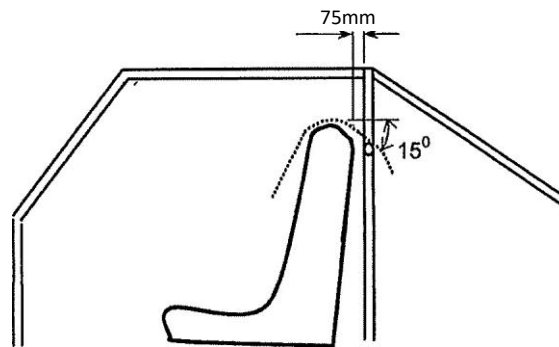
All vehicles to run adequate roll cage padding, to protect occupant's heads and body parts e.g. elbows.



(Diagram 1)

3.2-13 Seats and Restraints

- a) All seats must be securely mounted
- b) Recliner style seats shall have a restraining bar, no more than 75mm clearance between rear of seat and bar, to prevent rearward collapse of seat back.
- c) An approved harness is a requirement.
- d) Safety belts shall be fastened to seat belt frames or supports
- e) Mounting points to be secure to vehicle frame chassis.
- f) All belts to be approved manufacture
- g) Inertia reel belts not permitted
- h) Shoulder strap mounting points for conventional seats are to be positioned so that straps are not more than 15 degree downward angle from a horizontal plane extending from the occupants shoulder to the bar.



- i) All seats are to be fitted with headrests to reduce the chance of neck injury in front and rear collisions.
- . All seat belts to be manufactured to NZ Safety or approved Overseas Safety Standards. No seat belts to be modified or resewn.

3.2.14 Safety

As per Track Rules (see 2.1.23 Safety for Car, Driver and Passengers, Page 17)