

## 3.4 Modified Construction

### 3.4.1 Chassis

Chassis may follow builder's own design using appropriate materials (e.g. steel, alloys) but must include roll cage as part of chassis. A Speedway recognised chassis may run under roll cage rules applicable to their construction rules. **Modified Class shall race in an Anti Clockwise Direction.**

### 3.4.2 Roll Cage

Roll cage to have full 6 point roll cage to be securely welded to chassis. Main hoop/hoops to be 38mm O.D. x 3mm wall pipe, mild steel or stronger, with optional angle brace in hoop, lateral supports front and back, with front supports to have spreader bar across top of windscreen. All braces and supports to be 32mm O.D. x 3mm wall, all pipe sizes are minimum. Roll cage is at the scrutineers discretion. Chassis design must also be approved by the scrutineer.

### 3.4.3 Engines

The engine may be placed anywhere within the chassis provided the safety regulations are adhered to.

### 3.4.4 Transmission

The transmission may be set up in any way required. Transmission must be covered by a floor plate, with the bell housing covered by a scatter shield covering the top 180% degrees of the bell housing extending a minimum of 200mm from rear of engine block, made of ¼" plate or metric equivalent (6mm). Transmission is at Scrutineers discretion.

### 3.4.5 Differential

Once again the differential may be set up in any way desired, but the drive shaft must have a safety loop placed immediately behind the gearbox yoke to prevent the drive shaft digging into the track should the universal break.

### 3.4.6 Body

Body can follow any design desired but must at least contain a bonnet and firewall. The driver must be protected from all mechanical parts.

### 3.4.7 Battery

The battery may be placed within the driver's compartment, but must be completely covered by appropriate material e.g. battery box, and securely mounted. If placed elsewhere the battery may be exposed but must again be securely mounted.

### 3.4.8 Fuel Lines

Fuel lines must be of an approved material and securely and correctly installed. **No plastic lines or Plastic Fuel Filters.** No fuel lines in the driving compartment unless covered with approved material; e.g. aluminium or panel steel. Fuel caps must be screw on, or locking, and firmly secured. No rubber fuel caps permitted. If there is a glass pump bowl this must be protected to scrutineers satisfaction.

### **3.4.9 Bumpers**

This class may have front and back bumpers of diameter no more than 38mm (1 1/2") max or box section OD. Bumper must have capped ends.

### **3.4.10 Mufflers / Exhaust**

All exhaust systems must extend to the rear of the driver's seat, and face away from the driver. All participating cars are to run mufflers. Max 95db.

### **3.4.11 Carburettor / Throttle Body**

Two return springs to be fitted in a manner so as to close the throttle shaft.

### **3.4.12 Fire Extinguishers**

All cars or race vehicles to carry a fire extinguisher as per club specifications.

### **3.4.13 Methanol**

Cars running methanol are to display the letter "M" in fluorescent paint on their cars. The driver must notify the Head Scrutineer and the Clerk of the Course that they are running methanol.

All drivers must wear full length, long sleeve Proban Nomex blend (or material possessing the same protection rating) protective clothing with close fitting fronts, cuffs and ankles. The wearing of flame retardant gloves while driving in competition and practices is compulsory.

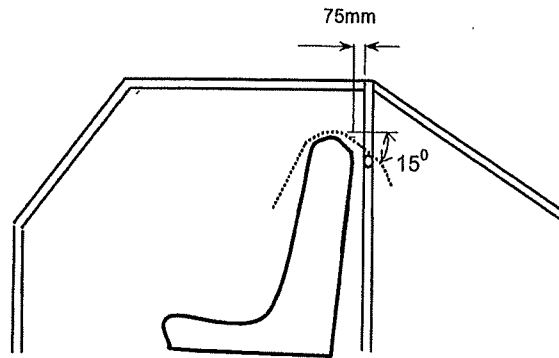
The use of flame resistant Balaclavas is optional.

### **3.4.14 Floor Pan**

All vehicles must have sufficient floor pan under the driving area e.g. from the pedals to the rear and covering the full width of the frame for driver/passenger protection.

### **3.4.15 Seats and Restraints**

- a) All seats must be securely mounted.
- b) Recliner style seats shall have a restraining bar no more than 75mm clearance between rear of seat and bar, to prevent rearward collapse of seat back.
- c) An approved motorsport harness is a requirement. All Harnesses to be 75mm and if a crutch strap is used a minimum of 45mm.
- d) Safety harnesses shall be fastened to seat belt frames or supports.
- e) Mounting points to be secured to vehicle frame chassis.
- f) All restraints to approved manufacture. All seat belts to be manufactured to N.Z. Safety or approved overseas Safety standards. No seat belts to be modified or resewn.
- g) Inertia reel belts are not permitted.
- h) Shoulder strap mounting points for conventional seats are to be positioned so that straps are not more than a 15 degree downward angle from a horizontal plane extending from the occupants shoulder, to the bar.



- i) All vehicles are to be fitted with headrests to help reduce the chance of neck injury in front and rear collisions.

### 3.4.16 Fuel Tanks

These may be moved, but must be isolated from the driver by a leak proof metal cover or firewall e.g. steel or aluminium, and secured.

If a rear mounted tank or "cell" is higher than shoulder height the firewall must be extended at least 25mm above the fuel tank or "cell".

### 3.4.17 Oil Lines, Water Pipes and Radiators

- a) All external oil lines are to be of approved oil resistant pressure hose securely fixed.
- b) All water pipes are to be securely fixed, and of approved type.
- c) Radiators are to be securely fixed and equipped with approved caps.
- d) All overflow pipes are to exit well away from the driver.

### 3.4.18 Nerf Rails

All open wheel race vehicles to be fitted with nerf rails which cover 80% of the rear tyre with 50% between front and rear wheels.

### 3.4.19 Kill Switch

A kill switch must be fitted within reach of the driver when belted in. The kill switch must stop the motor immediately when switched off. In the case of a magneto, a kill switch must also be fitted to kill the magneto.

### 3.4.20 Brakes

All vehicles to have adequate braking to the scrutineers discretion.

### 3.4.21 Racing Numbers

All cars must have their official racing number marked clearly on both sides of the car and on the roof with the bottom of the number facing right side of car and no less than 400mm in length. It is the competitor's responsibility to ensure that the competition numbers remain legible. While racing competition numbers must be clearly visible to officials and lap scorers, failure to comply may result in points not being awarded.